



Appendix Engaging Rolesville

The scale of bicycling is personal – usually, we have one person per bicycle independently controlling his or her own destiny. Speeds are slower than in an automobile, and the “feel” of the riding surface; the details of what we notice visually; and the sounds of approaching cars or the more pleasant sounds of a lawn mower, children laughing, or church bell make the experience of riding a bicycle special. In order to grasp these details, the experiences of other individuals that work, live, and play in a study area must be consulted. The public engagement plan for the Rolesville Comprehensive Bicycle Plan was designed to gather that detailed information, and emphasized five public involvement “projects:”

- Creation of a Steering Committee dedicated to seeing the Plan through to completion and offering their insights in a very detailed fashion;
- Development of a project website (www.rollininrolesville.com) that helped communicate events as well as preliminary information to the public and Steering Committee;
- Conducting two public events that would allow the general populace direct contact with the consultant and provide a venue for collecting information from the public;
- Creating surveys (2) that could be used in conjunction with the other methods to enhance consistent feedback from participants; and
- Presenting the Plan at local Planning Board and Town Council meetings during the review and adoption process to allow final comment by citizens as well as commentary from elected and appointed officials to incorporate into the final Plan document.

In addition, the draft Plan was submitted to the NCDOT Division of Bicycle and Pedestrian Transportation (DBPT) (and one drive-around review of the streets was conducted with Division Staff as well), Wake County, and the City of Raleigh and Town of Wake Forest, both of which share political boundaries with Rolesville. The following sections summarize the methods and results from each of these outreach methods and how the Plan responded to each of them.

A.1 Steering Committee

The Steering Committee met a total of three times over the course of the Project. Each meeting was advertised in advance with a published agenda emailed to each member and placed on the project website (refer to Section A.2, below). Meetings normally lasted two hours, and included a presentation by the Consultant followed by at least one



Appendix: Engaging Rolesville

"hands-on" exercise conducted with the Steering Committee. The impact of the Steering Committee was considerable: this group identified origins/destinations, vision/goals, and review project, program and policy recommendations at the same level of detail as the Town staff.

The following are the meeting summaries from each of the three meetings.

Summary - Steering Committee Meeting No. 1

April 30, 2012 | 6:30pm | Rolesville Community Center | 514 Southtown Circle | Rolesville | NC

Attendees (Affiliation)

Angie Coyle, Citizen (BB&T)
Patrick Delaney (Granite Falls Athletic Club)
J.G. Ferguson, Town of Rolesville (Parks & Recreation)
Gil Hartis - Town Board of Commissioners
Timothy Heilwig, Citizen (Open Space & Greenways Committee)
Mike Honkomp, Citizen (Parks & Recreation Advisory Board)
Thomas Lloyd, Town of Rolesville (Planning)
Scott Lane, Consultant (J. S. Lane Company, LLC)
Paul May, Citizen
Bob Mosher, NCDOT (Bicycle and Pedestrian)
Mark Powers, Town of Rolesville (Planning Board)
Tim Stoker, Town of Rolesville (Police Department)
Mike Szafran, Citizen (Wall Creek Neighborhood)
Alan Walker, Citizen (Wall Creek Neighborhood)

1. Introductions

Discussion: Please tell us about yourself, why you wanted to be a part of this Committee, and one hopeful outcome or goal of this project.

Mr. Lane and Mr. Lloyd welcomed the participants to the first Steering Committee meeting, and Mr. Lloyd noted that the project is funded in part by NCDOT through a grant. The participants went around the room and talked about early or current cycling experiences as well as what they would like to see developed as part of the planning process, which are bulleted below.



- Connect streets and other bicycle facilities together to create better connectivity with schools, parks, shopping and residences (+9)
- Better off-road facilities, like greenways and soft trails (+8)
- Coordinate the Bicycle Plan with other plans/policies as well as anticipated private sector developments (+8)
- Better on-road facilities, such as bicycle lanes, wide outside lanes and shoulders (+8)
- Increase cyclist safety(+5)
- Education to teach drivers and cyclists about proper behavior and safety (+4)
- Provide alternatives to single-occupant automobile travel (+2)
- Invest in long-term improvements (+1, -2)
- Increase awareness of the potential for cycling for fitness (-2)
- Better accommodations for long-distance riders (-5)

The participants were also asked later to rate their favorite objectives with white chips (one white chip = one point shown in parentheses) and their least favorite objectives (red chip = one negative point shown in parentheses above).

Mr. Mosher discussed the history of NCDOT's sponsorship of the bicycle and pedestrian grants, noting that over 100 bicycle or pedestrian plans had been created. The importance of the plan is to make sure that the community thinks about its cycling priorities, as well as to communicate those priorities accurately back to NCDOT.

2. Project Scope of Work

Discussion: The Consultant will walk us through the adopted scope of work, including the role of the Steering Committee and your responsibilities during the course of the project.

Mr. Lane reviewed the project scope of work for the project, and discussed projects, policies and programs, as well as how all three were needed. The current meeting would focus on the goals that the group would identify; future meetings would lay out suggested projects and review draft documents.

He continued by outlining the responsibilities of the Steering Committee, which would require approximately five meetings (one of which might be combined with a public workshop). The Committee's input would be required on goals, projects, programs, and policies, as well as prioritizing the many recommendations that would come from the Plan since there isn't enough money to address every need.



Appendix: Engaging Rolesville

3. Prioritizing Our Goals

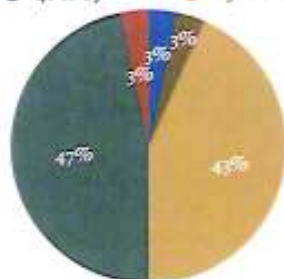
Discussion: We'll stretch our legs and prioritize our goals using a high-tech approach.

Mr. Lane reviewed various types of bicycle (e.g., road, mountain, hybrid, etc.) and types of cyclist (beginner, medium, and advanced) and how various types of facilities (bicycle lane, wide roadway shoulders, sharrow markings, etc.) as well as ancillary facilities (e.g., bicycle parking) accommodate these cyclists.

Mr. Lane asked each person to show how they would prioritize the objectives identified when they introduced themselves. (The results are shown in item one of this agenda.) Mr. Lane noted that in the next meeting they would review the results, as well as goals and a vision statement that would be created from these prioritized objectives as well as what we've learned from the preliminary survey.

4. Preliminary Results from the Public

Discussion: We've already been working at gathering input from the public through a survey distributed at the St. Patrick's Day (Bicycle) Parade, and its on-line counterpart (www.rollinginrolesville.com).



Mr. Lane reviewed the results from the survey, noting that additional surveys would potentially change the results.

← Of the 60 surveys received, none were from teenagers (11 to 17 years old); 90% were from people aged 26 to 65.

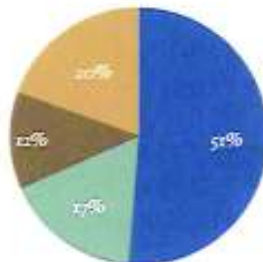
Appendix: Engaging Rolesville



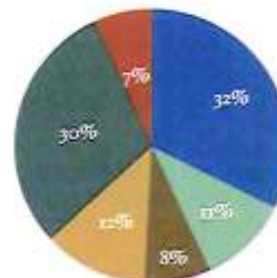
Most of the participants cited either (or both) accessing parks (30%) or cycling for exercise as their main reasons for riding a bicycle. Some people noted that reaching shopping (12%) or school (11%). →

● Park ● School ● Work
● Shopping ● Exercise ● Other

● 1-2 Times/Week ● 1-2 Times/Month
● 1-2 Times/Year ● Less than Once/Year

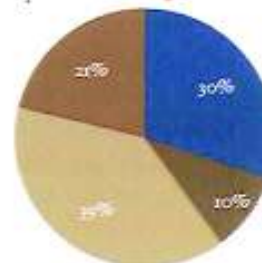


← When asked how frequently they ride, respondents mostly (51%) cited that they ride 1-2 times per week. However, one-fifth of the respondents cited that they rode less than once each year.



Respondents stated that the main reason that they don't ride more is because of a lack of connectivity (39%) to places that they want to go, and that cars travel too fast (30%). Other responses included not having a bicycle (10%) and miscellaneous responses (21%) that included aggressive dogs and a lack of time (three respondents). →

● Autos too Fast ● Don't Know How
● No Bicycle ● Unable to Ride
● No Easy Connections ● Other



The final questions dealt with helmet use – 62% of people asked said they wore a bicycle helmet. Of the 38% that don't wear a helmet, the main reason (79%) said that they



Appendix: Engaging Rolesville

don't own a helmet.

Some respondents also wrote in additional comments, which are listed below in no particular order.

- I would ride more should we have safer, better ways to ride distances and especially safe areas.
- Speeding is a huge problem in neighborhoods and on 401. I'm thinking that accidents will go up without a real crack down on speeders.
- Ecstatic about this news! This would be wonderful attraction for our "little" town.
- Would love to see Rolesville more bicycle friendly.
- I love riding my bike for exercise and definitely ride out on the back roads. My only concern is the traffic and that cars are not always "nice" to bike riders.
- This is a great idea for Rolesville. When the Bypass is finished, biking in Rolesville should be much easier, let's plan for that now, so things will be in place when the Bypass is completed. Also, connecting neighborhoods and parks with Greenway trails is a great amenity for any community.
- Would be great if you can ride and/or walk safely through all of Rolesville. Connect all the neighborhoods with Schools and Parks.

5. Future Workflow and Upcoming Tasks

Discussion: We'll talk about the best meeting days and times for you, and what we would like to do at our next meeting.

Mr. Lane emphasized the following items as upcoming work:

Complete the survey if you have not already done so.

Hand out blank surveys to at least five people you know.

Set date for second meeting 4-6 weeks from now.

The Consultant will collect data and prepare base mapping, as well as collect photographs and videos in anticipation of starting to identify facilities.

Other members of the group noted that two underpasses (culverts) had been designed into the Rolesville (401) Bypass to accommodate pedestrian and cyclist crossings.

Mr. Lane invited members to go on a small, casual group ride if they were interested, and suggested that they consider involving a school in some way, such as conducting a bicycle safety clinic. The law enforcement representative said that the Town hosts bicycle rodeos, but that attendance varies greatly.



Summary – Steering Committee Meeting No. 2

July 30, 2012 | 6:30pm | Rolesville Town Hall | 514 Southtown Circle | Rolesville | NC

Attendees (Affiliation)

Patrick Delaney (Granite Falls Athletic Club)
J.G. Ferguson, Town of Rolesville (Parks & Recreation)
Melissa Guilbeau, Consultant (Lagnilappe Planning)
Gil Harfis – Town Board of Commissioners
Timothy Hellwig, Citizen (Open Space & Greenways Committee)
Thomas Lloyd, Town of Rolesville (Planning)
Scott Lane, Consultant (J. S. Lane Company, LLC)
Paul D. May, Resident, Hampton Pointe Neighborhood
Mark Powers, Town of Rolesville (Planning Board)
Tim Stoker, Town of Rolesville (Police Department)
Mike Szafran, Citizen (Wall Creek Neighborhood)

1. Recap

Discussion: We'll talk about what we did at the first meeting and our current status in the project, and anything that you would like to see addressed in the Plan. This will become a standing item at our meetings.

Mr. Lane reviewed the work completed at the last Steering Committee meeting (April 30) and discussed the work that the Consultant Team had been doing in the interim (field surveys, drafting of existing conditions, base mapping).

Mr. Lane also asked the group what they considered as "must haves" in the Rolesville Bicycle Plan to make it a comprehensive success:

- Recreational riding facilities (e.g., greenways)
- Implementation and financing resources identified



Appendix: Engaging Rolesville

- Discuss bicycle lanes for running errands
- User-friendly bicycle systems and facilities
- Safety action plan
- Physical space to ride in (with cars)
- Explore grant monies for financing construction
- Create a modern, complete plan for the Town
- Create projects that are competitive for financing in the eyes of NCDOT
- Safe facilities for the "not-so-hardcore" riders
- Create programs to encourage safe bicycle riding for both motorists and pedestrians

2. Review of the First Plan Sections

Discussion: We'll distribute and discuss the Plan sections, which include an overview, existing conditions and (draft) visions/goals for the Plan.

Mr. Lane reviewed the draft sections of the Plan presented in the Committee's binder: reasons for cycling, public opinion, existing conditions on major facilities, and the vision/goals for the Plan. He further explained that his overview was simply to provide them with an introduction to the content this evening, not to replicate the full contents of the draft report.

The group discussed the important of having roadways wide enough to accommodate parents with kids in trailers; double-file riding groups; and how wide outside lanes may stay cleaner due to the passing of cars (as compared to bicycle lanes). Mr. Lane noted that some neighborhoods did not use on-street parking while others used it extensively; the group noted that some neighborhood associations may prohibit on-street parking. The only known bicycle parking apart from schools was located at Main Street Park.

Mr. Lane discussed how the Vision and Goals were produced, noting that the keywords developed by the Steering Committee at their first meeting were instrumental in generating both the goals and the vision statement for the Plan. The goals of the Rolesville Bicycle Plan are:

- Our Town will be **better connected** and accessible by bicycle than it is today.
- Our Town will feature **on-road bicycle facilities that connect us** to places both within and near our borders in part to provide alternatives to making every trip with a car.
- Our Town will **grow our greenway and trail system**, and dedicate time and resources to that end.



- Our Town will **engage our residents proactively** to ensure that everyone – motorists and cyclists alike – will be respectful and aware of each other to ensure the safety of every cyclist.
- Our Town will **consider bicycling and bicycle accommodations** in every new development review, policy, ordinance, and resolution adopted.

The Vision Statement is:

Rolesville will be a Town where it is safe to ride a bicycle both on and away from the roads as part of an integrated policy framework and transportation system that connects us with each other and the places we want to reach.

3. Developing Projects

Discussion: The committee will break into 2-3 "teams" to identify origins and destinations on maps

(This item was combined with agenda item number four.) Mr. Lane reviewed some of the information from the previous meeting concerning the purpose of different bicycle facility types, noting that the group would be suggesting on- and off-road solutions to connecting origins and destinations this evening. Mr. Lane also led a discussion with the group concerning the death of Steve Jordan, a cyclist struck and killed on US 401 south of Rolesville on July 4th. The group noted that no treatment could make US 401 very safe, although wider outside lanes, adjacent sidepaths (although with concerns about driveway conflicts), and a separate path were addressed as alternatives. Mr. Lane also led the group in a safety discussion with four true/false questions concerning the most recent facts on cycling fatalities and accident statistics.

The group prepared a map showing origins (green dots), destinations (red dots) and on-road (red lines) and off-road (green lines) connections. [Figure 1](#) on the following page illustrates the outcome of these initial recommendations. Many of the destinations were along Main Street. One group focused more on destinations outside of Rolesville and reaching them via the major arterials, while the other group focused more on the core of Rolesville and building both on- and off-road facilities to reach them.



Appendix: Engaging Rolesville

4. More About Facility Types

Discussion: Mr. Lane will discuss the different kinds of bicycle facilities, as well as ways of improving safety.

(This item was combined with agenda item number three.)

5. Future Workflow and Upcoming Tasks

Discussion: We'll discuss the upcoming policy and program elements of the draft Plan.

Mr. Lane briefly discussed the upcoming work of the Committee, noting that preliminary project recommendations, policy and program discussions would be the focus of the third Steering Committee Meeting. He also asked the group to consider public venues to present the draft Plan when it became ready.

The meeting adjourned at 8:20pm.

Summary – Steering Committee Meeting No. 3

September 10, 2012 | 6:30pm | Rolesville Board Chambers | 502 Southtown Circle | Rolesville | NC

1. Recap

Discussion: We'll discuss what we accomplished at the second meeting, and move into the recommendations that we considered for facilities at this meeting.

No one presented any major new issues from the prior meeting or notes.



2. Review of the Physical (Project) Recommendations

Discussion: We'll discuss the project recommendations and make any additional adjustments.

Most comments created by the Committee related to the proposed improved routes. The Committee did have a couple of additional locations for bicycle racks that are in place. Mr. Mosher noted the west side of town and lack of north-south connectivity for a bicycle route considering the east-west routes is 45 mph speed limits and there is a draw with the school complex. The group discussed the issue and noted that the development characteristics in the middle of that area stem from an old 1970s county subdivision where the parcels are not likely to redevelop in a coordinated manner.

The group discussed the utility corridor on the south side of town and potential to become either a greenway or location for some mountain bike trails. One member of the group mentioned that the utility company has caught people walking on it and that they are not happy when that happens.

Main Street: The Committee expressed a desire to develop two tiers of recommendations - 1) Short-term improvements for re-striping, signage, etc.; 2) Long-term more substantial improvements as noted in the design plan in the document. Seek a pre- / post study on the route prior to the bypass opening to gauge volumes, turning movements, speeds, other characteristics, perhaps via CAMPO and/or NE Area Study that is happening shortly.

Mr. Lloyd noted that all of the Town's development policies hinge on the impacts of the bypass, saying it feels like the Town is in limbo until that opens.

3. Developing Programs and Policies

Discussion: The committee will review suggested programs and policies developed and presented by the consultants.

The Committee liked the idea of recommending that the Town organize an annual bicycle event, noting the criterium in Wake Forest or similar community rides (10 mi., 30 mi., 60 mi. distances to accommodate different skill sets of riders).

The Committee also mentioned the idea of having a walk/bike/run event on the US 401 Bypass before it



Appendix: Engaging Rolesville

officially opens; perhaps a Ciclovía-type event on Main Street annually when Bypass opens. Mr. Kostelec noted that there is a missing link in education for greenway use: we have bicycle rodeos for kids and TS 101 for adults focused on road-riding. The greenway system needs to have a comparable educational component.

Also noted was the fact that Asheville has a program where they purchased bicycle lights for police officers to give to people who are riding at night without lights. The Committee also favored this idea.

Mr. Lloyd noted that the Town does have an Overlay District in place similar to what is recommended.

4. Future Workflow and Upcoming Tasks

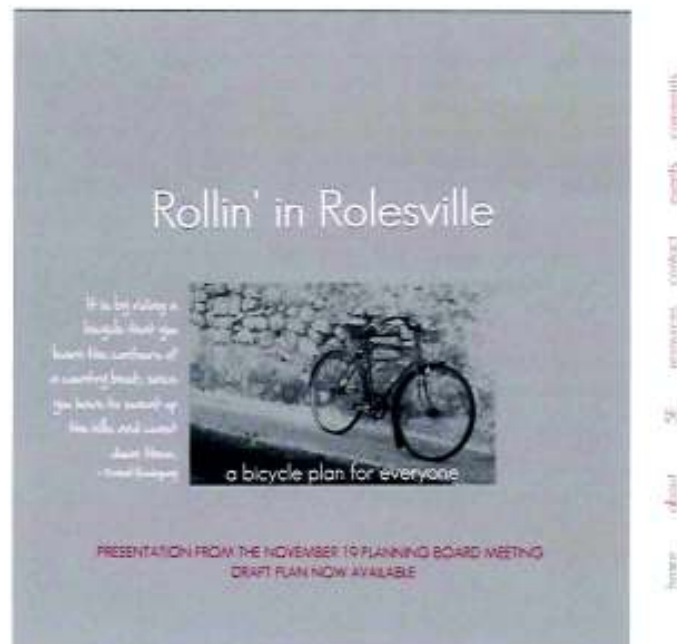
Discussion: We'll discuss how to advertise the plan and its remaining pieces.

When asked about the potential for an upcoming festival or other event to introduce the draft Plan for comments, the Committee made several suggestions. The Committee noted the Fall Festival and the Chamber's 5k/10k event as potential events for outreach. They also suggested tying into the HOAs, which have meetings and newsletters that cover a large number of the residents. There is a Saturday morning bike ride out of Heritage and a Triathlon group that meets by Granite Falls. Either of these groups could be instrumental in advertising the Plan.



A.2 Project Website

The project website (www.RollinInRolesville.com) was constructed in the first month of the Project and maintained throughout the planning process. It was active for approximately one year; January 2012 through January 2013. The activities included posting a video/simulation; meeting agendas and summaries; project resources and objectives; contact information; and preliminary draft plan components. A blog page was also included but not heavily utilized, although the Consultant did note major occurrences in this location. The utility of the project website was mainly to communicate information to the Steering Committee and serve as a document repository.





Appendix: Engaging Rolesville

A.3 Public Events

Apart from the actual Plan adoption process by the Town of Rolesville (see Item A.5, below), two public events were attended in an attempt to gather direct information from the public. These two venues were chosen instead of a dedicated public meeting because (a) the events have had good historical participation; and (b) surveys and other information could be created that fit into the overall event framework. These events shaped the Plan directly by influencing projects such as the crossing treatment on Main Street near the Elementary School and Park, and indirectly by generating survey data that was used to inform the Steering Committee and influence project priorities.

The **St. Patrick's Day Bicycle Parade** held on July 12, 2012 was especially relevant since children and adults were invited to bring their bicycles to the event. This event is popular in Rolesville, and is actually mentioned in the program section of the Plan in terms of ways of enhancing the benefits and goals of the Bicycle Plan. A member of the consulting team was present to distribute postcard-style surveys to people attending the event, and over 60 surveys were collected this way. People that completed and returned a survey were eligible to win a \$50 gift certificate at a local restaurant as well.



The **Fall FunFest** event was held on November 5, 2012. Since the atmosphere at this event was more oriented towards a crafts fair, a booth was set up by the Town and Consultant to engage people in learning more about the Plan and to provide a venue to offer their comments. A digital survey kiosk, looping summary video, five copies of the draft Plan, and draft Recommendations poster were on display. Staff also conducted demonstrations of a bicycle blender, and offered free smoothies to anyone that completed a survey. Approximately 20 surveys were completed at this event.





A.4 Survey Results

Since two versions of the survey were distributed at two different times but contained several questions that were the same or similar, the results of the surveys are presented as one. The maximum number of respondents was 87 for any question.



Check the one answer that best describes how often you bicycle.

At least 1-2 times per week	33	38%
At least 1-2 times per month	22	25%
At least 1 to 2 times per year	15	17%
Less than once a year	17	20%



What prevents you from riding a bicycle more often?

Automobile traffic is too fast	39	34%
Don't know how to ride	1	1%
Don't have a bicycle	10	9%
Unable to ride a bicycle	4	3%
Not easy connections to places I want to go	40	35%
Other	21	18%



Check the places where you bike now or would like to ride a bicycle.

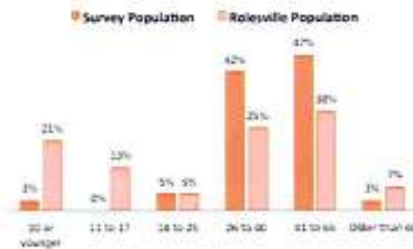
Park	57	35%
School	14	8%
Work	12	7%
Shopping	21	13%
Exercise	49	30%
Other	12	7%



Appendix: Engaging Rolesville

Please tell us your approximate age.	Survey Population	Rolesville Population*
10 or younger	3%	21%
11 to 17	0%	13%
18 to 25	5%	5%
26 to 40	42%	25%
41 to 65	47%	30%
Older than 65	3%	7%

*Presented for comparative purposes; source: 2010 U.S. Census



Do You Think that If the Plan Were Implemented, It Would Make Bicycling Better/Easier/Safer than It is Right Now? (n=21*)

No! This Plan Won't Help at All	1	5%
Not Sure...	2	10%
Yes, This Plan Will Make Biking Somewhat Better	3	14%
Yes! This Plan Will Make Biking a Lot Better	15	71%



Note: This question was only asked of the 21 people who took the survey at the Fall FunFest after the Draft Plan had been completed.



A.5 Plan Review and Adoption

The Rolesville Comprehensive Bicycle Plan was reviewed by the Planning & Zoning Board on November 19, by the Town Commissioners on February 4, 2013; and again for a public hearing on February 19th. At the November 19th meeting, Thomas Lloyd, Planning Director, presented the Plan to the Planning & Zoning Board, noting that the Plan contained recommendations for physical improvements as well as program and policy changes. Mr. Westbrook, Member of the Board, asked about bicycle lanes and paths, and asked who has the final authority on the Plan. Mr. Lloyd responded that additional changes could be made even after the Town adopts the Plan by a vote of Town Commissioners.

The Town Commissioners heard a presentation by Scott Lane, Project Manager of the consulting team for the Plan, which outlined the public engagement process, purposes, and recommendations of the planning process. Mr. Lane acknowledged the work of the Town staff as well as the Steering Committee that reviewed and provided recommendations for the Plan. Mr. Lane responded to inquiries concerning the use of a HAWK signal treatment at the site of the Main Street Park and elementary school, citing that this novel treatment (Mr. Hicks showed an image of such a signal to help illustrate the description) was warranted due to the proximity of these two primary locations along the main street of Rolesville. Several commissioners indicated that they would like to bicycle more themselves or have their children/grandchildren do so, but are prevented by concerns about the speed and volume of traffic. Commissioner Whitley motioned to schedule a public hearing on Tuesday, February 19; Commissioner Hartis seconded the motion, which passed unanimously.

